









**LEGEND**

-  SECTION/DETAIL IDENTIFICATION REFERENCE DRAWING NO.
-  EXISTING FACILITY (I.E. CURB/GUTTER/PARKING STRIP)
-  PROPERTY LINE
-  CATCH BASIN
-  DIRECTION OF DRAINAGE FLOW
-  PULL BOX
-  FIRE/POLICE CALL BOX
-  PERPENDICULAR

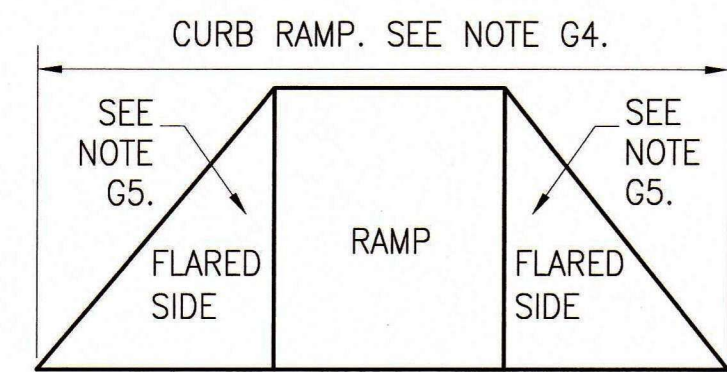
**ABBREVIATIONS**

- ACWS = ASPHALT CONCRETE WEARING SURFACE
- B = BACK OF CURB
- BOC = BOTTOM OF CURB
- BSW = BACK OF SIDEWALK
- C = CURB
- CB = CATCH BASIN
- DAC = DISABILITY ACCESS COORDINATOR
- DW = DETECTABLE WARNING
- DWG = DRAWING
- (E) = EXISTING
- EL = ELEVATION
- FIG = FIGURE
- FL = FLOW LINE
- FOC = FACE OF CURB
- G = GUTTER
- GB = GRADE BREAK
- MAX. = MAXIMUM
- MIN. = MINIMUM
- (N) = NEW
- PAR = PEDESTRIAN ACCESS ROUTE
- PED = PEDESTRIAN
- PL = PROPERTY LINE
- PL = RIGHT OF WAY
- R.O.W. = RIGHT OF WAY
- TOC = TOP OF CURB
- TYP. = TYPICAL
- UON = UNLESS OTHERWISE NOTED

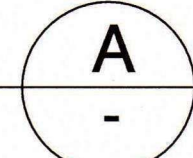
**GENERAL NOTES**

- G1. IT IS THE PURPOSE OF THESE DOCUMENTS TO PROVIDE TECHNICAL STANDARDS THAT EXPAND ON THOSE FOUND IN FEDERAL, STATE, AND LOCAL ACCESSIBILITY REGULATIONS. THE SCOPE OF ACCESSIBLE FEATURES (LOCATIONS REQUIRED) IS SET FORTH IN THE FEDERAL, STATE, AND LOCAL ACCESSIBILITY STATUTES AND REGULATIONS. SCOPING OF THE DEPICTED ELEMENTS IS NOT THE PURPOSE OF THESE DOCUMENTS.
- G2. THE ACCESSIBLE STREET CROSSING STANDARD, FILE NO. 102,854 THROUGH 102,864 SUPERSEDE ALL PREVIOUS STANDARD CURB RAMP DRAWINGS FILE NO. 55,017 THROUGH 55,018.3. ALL WORK SHALL CONFORM TO SF PUBLIC WORKS STANDARD PLANS AND SPECIFICATIONS, LATEST VERSIONS.
- G3. STANDARD CURB RAMP LAYOUTS SHOWN ON RX-3 THRU RX-6 ARE TO BE USED IN NEW CONSTRUCTION AND IN ALTERATIONS (SEE NOTE G39) TO THE EXTENT IT IS TECHNICALLY FEASIBLE OR STRUCTURALLY PRACTICABLE. USE OF SHEETS RX-7 TO RX-11 IN ALTERATIONS OF (E) PUBLIC R.O.W. ARE SUBJECT TO APPROVAL BY CITY ENGINEER, PUBLIC WORKS DISABILITY ACCESS COORDINATOR (DAC), OR THEIR DESIGNEE, AND REQUIRES DOCUMENTED EVIDENCE THAT STANDARD CURB RAMP CONSTRUCTION WOULD BE TECHNICALLY INFEASIBLE OR STRUCTURALLY IMPRACTICABLE. CURB RAMP LAYOUT SHEETS ARE ORDERED FROM HIGHEST TO LOWEST ALLOWABLE DESIGN. WITHIN EACH SHEET, DETAILS ARE ORDERED FROM HIGHEST TO LOWEST ALLOWABLE LAYOUT, UNLESS OTHERWISE NOTED. THE HIGHEST ORDERED LAYOUT THAT FITS WITHIN SITE CONSTRAINTS SHALL BE PROVIDED.
- G4. A "CURB RAMP" IS DEFINED AS THE AREA WHICH INCLUDES THE RAMP AND THE FLARED SIDES. THE "RAMP" IS DEFINED AS THE MINIMUM 4-FOOT WIDE CENTER PORTION INCLUDING THE DETECTABLE WARNING ("DW"). THE "FLARED SIDE" IS DEFINED AS THE AREA ON EITHER SIDE OF THE RAMP WHICH LIES ON A SLOPED PLANE. (SEE DETAIL "A.") THE CURB RAMP SURFACES SHALL BE PLANAR WITH A MINIMUM DEGREE OF WARPING AND A SURFACE PLANARITY (FLATNESS TOLERANCE) OF 3/8 INCH PER 10-FOOT STRAIGHT EDGE MAXIMUM.
- G5. SIDES OF CURB RAMPS: IF A CURB RAMP IS LOCATED WHERE PEDESTRIANS MAY WALK ACROSS THE RAMP, OR WHERE IT IS NOT PROTECTED BY HANDRAILS OR GUARDRAILS, IT SHALL HAVE FLARED SIDES. CURB RAMPS WITH RETURNED CURBS MAY BE USED WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP SUCH AS AT LANDSCAPED AREAS.
- G6. A LEVEL LANDING CONCRETE GUTTER OF 2 FEET MINIMUM WIDTH SHALL BE PROVIDED AT THE LOWER END OF THE RAMP. THE ALGEBRAIC SUM OF THE OPPOSING SLOPES BETWEEN TWO ADJACENT SURFACES SHALL NOT EXCEED 10.33%. THE BOTTOM OF RAMP SHALL BE FLUSH WITH LOWER LANDING (NO LIP). PROVIDE GUTTER CONTROL JOINTS AT FRONT OF RAMP AND ALIGN WITH DIRECTION OF RAMP.
- G7. A LEVEL LANDING 4 FEET MINIMUM IN LENGTH SHALL BE PROVIDED AT THE UPPER END OF EACH CURB RAMP.
- G8. EXISTING VERTICAL OBSTRUCTIONS SHALL HAVE A MINIMUM CLEARANCE OF 6-INCHES FROM EDGE OF RAMP MEASURED FROM THE NEAREST PROTRUDING PART BETWEEN A HEIGHT OF 8-INCHES AND 80-INCHES.
- G9. PEDESTRIAN PUSHBUTTON POLES WILL ONLY BE ALLOWED IN THE "PEDESTRIAN PUSHBUTTON ALLOWABLE AREA" AS NOTED ON SHEET RX-5, DETAIL A1 AND B1, WITH FACEPLATE ORIENTED PARALLEL TO CROSSWALK PATH OF TRAVEL, AND PUSHBUTTON ON SIDE CLOSEST TO RAMP. ALTERNATE LAYOUTS FOR PEDESTRIAN PUSHBUTTON POLES MAY BE POSSIBLE IN CASES OF TECHNICAL INFEASIBILITY PROVIDED EQUAL FACILITATION IS PROVIDED PER MUTCD GUIDANCE.
- G10. FIRE AND POLICE CALL BOXES: SEPARATION BETWEEN CURB RAMPS (CLEARANCE "A") SHALL BE 3' MIN. WIDTH FOR WHEELCHAIR ACCESS TO CALL BOX. 2.5' ALLOWABLE AT CLEARANCE "A" IF CALL BOX IS SET LESS THAN OR EQUAL TO 2' FROM TOP OF CURB RAMP. CALL BOXES MAY BE LOCATED BETWEEN RAMPS IF CLEARANCE "A" IS GREATER THAN OR EQUAL TO 2.5'. IF CLEARANCE "A" LESS THAN 2.5', CALL BOXES MAY BE PLACED IN "PEDESTRIAN PUSHBUTTON ALLOWABLE AREA."
- G11. WHEN RAMP IS NOT PERPENDICULAR TO CURB, LONG FLARED SIDE CAN BE TRUNCATED AS INDICATED ON SHEET RX-5, DETAIL B.
- G12. THE SURFACE OF THE CURB RAMP AND DW (INCLUDING DOME TOPS AND BETWEEN DOMES) SHALL BE STABLE, FIRM, AND SLIP RESISTANT. THE CONCRETE CURB RAMP SURFACE SHALL BE BROOM FINISHED TRANSVERSE TO THE AXIS OF THE RAMP AND SHALL BE SLIGHTLY ROUGHER THAN THE FINISH ON THE ADJACENT SIDEWALK SURFACE. SLIP RESISTANCE SHALL BE MEASURED IN ACCORDANCE WITH PUBLIC WORKS ORDER 176,112 OR CURRENT SUBSEQUENT ORDER.
- G13. WHEN THE 4-FOOT MINIMUM DISTANCE BETWEEN FLARED SIDES OF TWO ADJACENT CURB RAMPS CANNOT BE ACHIEVED DUE TO TECHNICAL INFEASIBILITY, DISTANCE BETWEEN MAY BE REDUCED. SEE SHEET RX-7, NOTE 4.
- G14. CURB RAMPS SHALL HAVE A STRONG VISUAL CONTRAST WITH ADJACENT SIDEWALK AND ARE TO BE POURED SEPARATELY. DARK COLORED CURB RAMPS SHALL BE USED IN AREAS WITH CITY STANDARD SIDEWALK AS DEFINED IN SECTION 204 OF PUBLIC WORKS STANDARD SPECIFICATIONS. CURB RAMPS TO BE CITY STANDARD SIDEWALK COLOR WHEN ADJACENT SIDEWALK IS WITHIN DOWNTOWN STREETScape PLAN (DARK SIDEWALK IN C-3 DISTRICTS), AS DEFINED IN PUBLIC WORKS ORDER NO.172,596. THE FOLLOWING MANUFACTURERS AND COLOR, OR APPROVED EQUAL, MAY BE USED FOR FADE RESISTANT DARK COLOR: (1) L.M. SCOFIELD "C-24 CHARCOAL GRAY"; (2) QC INTEGRAL COLORS "IC-3 ASH GRAY"; (3) SOLOMON COLORS "ONYX 920". CARBON BLACK SHALL NOT BE USED FOR DARK PIGMENT. GROOVED BORDER MAY BE OF SIDEWALK OR CURB RAMP COLOR. GROOVED BORDERS AT ADJACENT CURB RAMPS SHALL MATCH.
- G15. CURB RAMP LENGTH MAY BE LIMITED TO 15 FEET WHERE EXISTING SIDEWALK OR ROADWAY RUNNING GRADE MAKES IT TECHNICALLY INFEASIBLE OR STRUCTURALLY IMPRACTICABLE TO LIMIT RAMP SLOPE TO 8.33%
- G16. "CROSSWALK," AS DEFINED IN UNIFORM VEHICLE CODE, SECTION 1-112, IS EITHER: "(A) THAT PART OF A ROADWAY AT AN INTERSECTION INCLUDED WITHIN THE CONNECTIONS OF THE LATERAL LINES OF THE SIDEWALKS ON OPPOSITE SIDES OF THE HIGHWAY MEASURED FROM THE CURBS, OR IN THE ABSENCE OF CURBS, FROM THE EDGES OF THE TRAVERSABLE ROADWAY; AND IN THE ABSENCE OF A SIDEWALK ON ONE SIDE OF THE ROADWAY, THE PART OF A ROADWAY INCLUDED WITHIN THE EXTENSION OF THE LATERAL LINES OF THE EXISTING SIDEWALK AT RIGHT ANGLES TO THE CENTERLINE. (B) ANY PORTION OF A ROADWAY AT AN INTERSECTION OR ELSEWHERE DISTINCTLY INDICATED FOR PEDESTRIAN CROSSING BY LINES OR OTHER MARKINGS ON THE SURFACE."
- G17. PUBLIC WORKS POLICY IS THAT RAMP IS TO RUN PARALLEL TO CROSSWALK AND BE PLACED AS CLOSE TO CENTER OF CROSSWALK AS FEASIBLE. WHERE CROSSWALK MARKINGS EXIST, FULL WIDTH OF RAMP IS TO LIE WITHIN CROSSWALK MARKINGS WITH A PREFERRED MINIMUM OF 3 FEET BETWEEN RAMP AND MARKINGS. ALIGNMENT OF RAMP AT OPPOSITE SIDES IS DESIRABLE, ESPECIALLY AT WIDE CROSSWALKS. EACH CROSSWALK SHALL HAVE A CURB RAMP AT EACH END.
- G18. PEDESTRIAN CROSSWALKS, AS REFERENCED IN THESE STANDARDS, ARE SHOWN FOR GENERAL LOCATION ONLY. ACTUAL CROSSWALK MARKINGS SHALL BE PER SFMTA STANDARDS WHERE MARKED.
- G19. CURB HEIGHT WITHIN CROSSWALK AREA SHOULD BE IN ACCORDANCE WITH PUBLIC WORKS STANDARD PLANS (6-INCH TYPICAL). WHERE NECESSARY, TO PROVIDE CURB RAMPS, CURB HEIGHT MAY BE 4 TO 7-INCHES WITHIN CROSSWALK AND CURB RETURN AREA. CURB HEIGHT OVER 7-INCHES MAY BE APPROVED ON A CASE BY CASE BASIS BY CITY ENGINEER, PUBLIC WORKS DAC, OR THEIR DESIGNEE, OUTSIDE OF CROSSWALK AREAS (FOR EXAMPLE WITHIN RED CURB AREAS.) CURB HEIGHTS <6" MAY BE APPROVED AS A CASE BY CASE EXCEPTION BY PUBLIC WORKS HYDRAULIC SECTION MANAGER.

- G20. PER CALIFORNIA VEHICLE CODE (CVC) 22522, "NO PERSON SHALL PARK A VEHICLE WITHIN THREE FEET OF ANY SIDEWALK ACCESS RAMP CONSTRUCTED AT, OR ADJACENT TO, A CROSSWALK OR AT ANY OTHER LOCATION ON A SIDEWALK SO AS TO BE ACCESSIBLE TO AND USABLE BY THE PHYSICALLY DISABLED, IF THE AREA ADJOINING THE RAMP IS DESIGNATED BY EITHER A SIGN OR RED PAINT." TO MEET CVC 22522 REQUIREMENTS, CURB IS TO BE MARKED WITH RED PAINT TO DESIGNATE NO PARKING WITHIN 3' OF OUTSIDE EDGE OF RAMP. FOR RAMPS WHERE PATH OF TRAVEL IS NOT PERPENDICULAR TO THE CURB, CURB SHALL BE PAINTED RED AT THE MINIMUM LENGTH NEEDED TO DESIGNATE NO PARKING TO MAINTAIN 3' CLEARANCE BETWEEN OUTSIDE EDGE OF RAMP AND FULL WIDTH/FARTHEST CORNER OF PARKED VEHICLE, OR MARKED VEHICLE PARKING SPACE. CURB MARKINGS ARE NOT TYPICALLY REQUIRED WITHIN MARKED CROSSWALKS; THEREFORE, CURB SHALL BE PAINTED RED WHERE PARKING RESTRICTED AREA OF CURB FALLS OUTSIDE CROSSWALK MARKINGS. SFMTA PROVIDES ALL CURB MARKINGS. MARKING TO BE COORDINATED WITH SFMTA.
- G21. EXISTING UTILITY BOXES AND COVERS SHALL BE ADJUSTED TO BE FLUSH WITH CURB RAMP SURFACE AND SHALL NOT STRADDLE ANY CHANGE IN PLANE. CONTACT UTILITY OWNER FOR INSTRUCTIONS ON PROPER ADJUSTMENT OF AT-GRADE FACILITIES. NO UTILITY BOXES AND COVERS ARE TO LIE WITHIN THE DW AREA. WHERE CURB RAMPS CANNOT BE DESIGNED TO AVOID EXISTING WATER VALVES, EXISTING WATER VALVES SHALL BE RELOCATED OUTSIDE OF CURB RAMPS. NO WATER METER BOXES GREATER THAN 10" X 15" SHALL BE IN FLARED SIDES. NEW UTILITY BOXES SHALL NOT BE PLACED WITHIN THE GROOVED BORDER, CURB RAMP, OR CURB RETURN AREA. SEE PUBLIC WORKS ORDER 175,387.
- G22. THE CONTRACTOR SHALL REFERENCE AND PRESERVE ANY EXISTING MONUMENTS WITHIN THE LIMITS OF WORK. A CORNER RECORD OR RECORD OF SURVEY SHALL BE FILED WITH THE COUNTY SURVEYOR PURSUANT TO THE CALIFORNIA BUSINESS AND PROFESSIONS CODE, SECTION 8771, PRIOR TO ANY WORK COMMENCING. IF ANY MONUMENT IS DESTROYED, DAMAGED, COVERED, OR OTHERWISE OBLITERATED, THE CONTRACTOR SHALL RESET SAID MONUMENT AS REQUIRED AND MANDATED IN PUBLIC WORKS DOCUMENT "MONUMENT PRESERVATION." A COPY OF THIS DOCUMENT MAY BE OBTAINED FROM THE OFFICE OF THE COUNTY SURVEYOR, 1155 MARKET STREET, 3RD FLOOR, SAN FRANCISCO, CA 94103, TELEPHONE: 415-554-5810.
- G23. THE STREET NAME SHALL BE STAMPED IN BOLD UPPERCASE LETTERS 4-INCH HIGH AND 1/2-INCH DEEP ON THE FLARED SIDE FARTHEST FROM ANGULAR CORNER OR ON AN ADJACENT SIDEWALK FLAG AS DIRECTED BY THE ENGINEER. NAME SHALL BE PARALLEL TO THE STREET IT NAMES.
- G24. RAMPS WITH GRADE BREAKS SHALL HAVE A BACK OF CURB ELEVATION OF 0.01 FEET HIGHER THAN THE SPECIFIED FACE OF CURB ELEVATION.
- G25. RAMPS WITHOUT GRADE BREAKS MAY HAVE A MAXIMUM BACK OF CURB ELEVATION OF 0.03 FEET HIGHER THAN THE FACE OF CURB ELEVATION.
- G26. AT LOCATIONS WHERE IT IS NOT TECHNICALLY FEASIBLE TO DESIGN TO THE SLOPES ON SHEETS RX-3 AND RX-4, LESSER OR GREATER SLOPES MAY BE APPROVED ON A CASE BY CASE BASIS BY CITY ENGINEER, PUBLIC WORKS DISABILITY ACCESS COORDINATOR, OR THEIR DESIGNEE. AS MUCH CONSTRUCTION TOLERANCE AS IS FEASIBLE SHALL BE MAINTAINED, EVENLY APPORTIONED BETWEEN THE LANDING AND THE RAMP. SPECIFYING A DESIGN RUNNING SLOPE OF 7.5% MAXIMUM AND A CROSS SLOPE OF 1.5% MAXIMUM FOR CURB RAMPS WILL ACCOMMODATE MOST IRREGULARITIES OR VARIANCES DUE TO CONSTRUCTION METHODS OR MATERIALS.
- G27. THE DESIGN CROSS SLOPE AT THE SIDEWALK SHALL BE A MINIMUM OF 1% TO A MAXIMUM OF 1.5% (0.5% MINIMUM TO 2% MAXIMUM CONSTRUCTED). RUNNING GRADE OF SIDEWALK SHALL BE ALLOWED TO MATCH RUNNING GRADE OF ADJOINING STREET.
- G28. CROSS SLOPE IS DEFINED AS SLOPE PERPENDICULAR TO PEDESTRIAN PATH OF TRAVEL AT ANY GIVEN POINT.
- G29. WHERE CURB RAMP WIDTHS AND UPPER LANDING LENGTHS ARE INDICATED AS 4', THE DIMENSION SHALL BE 4'2" WHEN LOCATED WITHIN CALTRANS R.O.W.
- G30. CONFORM BETWEEN EXISTING ELEVATIONS AND NEW CURB AND NEW SIDEWALK IN CURB RETURN AREA. CONFORM MAY REQUIRE ADDITIONAL CURB AND SIDEWALK BEYOND END OF RAMP. EXISTING SIDEWALK FLAGS IMMEDIATELY ADJACENT TO SIDEWALK AND CURB AREA TO BE REPLACED, EXCEPT OVER SUBSIDEWALK BASEMENTS, SHALL BE REMOVED TO NEAT FLAG LINES (CONTROL JOINTS) TO ENSURE THAT SIDEWALK WILL CONFORM SMOOTHLY WITHOUT ABRUPT CHANGES IN SLOPE. EXTENDED GUTTER MAY BE NECESSARY DEPENDING ON SLOPE.
- G31. WHERE GUTTER FLOW LINE SLOPES DOWN AND AWAY FROM CURB RAMP LOWER LANDING, FLOW LINE SLOPES ADJACENT TO THE LEVEL LANDING AT THE GUTTER SHALL NOT EXCEED 5% FOR THE FIRST 1' (MINIMUM) TO 3' (PREFERRED) OF LENGTH. FLOW LINE SLOPES BEYOND 1' TO 3' OF THE CURB RAMP LOWER LANDING MAY EXCEED 5% BUT SHOULD NOT EXCEED 8% WITHIN THE CROSSWALK AREA WHERE FEASIBLE.
- G32. POSITIVE DRAINAGE IS REQUIRED TO DRAIN SURFACE WATER ON SIDEWALKS AWAY FROM PROPERTY LINES AND BUILDINGS TO THE STREET OR OTHER STORMWATER CONVEYANCE OR STORAGE.
- G33. BASE REPAIR IS REQUIRED WHEN THE OUTSIDE CONCRETE GUTTER ELEVATION IS LOWERED MORE THAN 1/2-INCH OR RAISED MORE THAN 2-INCHES WHERE EXISTING ACWS IS STANDARD 2-INCH THICK.
- G34. REFER TO PUBLIC WORKS STANDARD PLAN 96,608, REV. 1, FOR PLACEMENT OF STEEL REINFORCED BARS AT ALL RE-ENTRANT CORNERS (PROPERTY CORNERS, CURB RAMPS, PULL BOXES, ETC.) IN SIDEWALK CONSTRUCTION.
- G35. PEDESTRIAN ACCESS ROUTE (PAR) CONSISTS OF SIDEWALKS, LEVEL LANDINGS AT UPPER END OF RAMPS, LANDINGS AT BOTTOM OF RAMPS AT GUTTER, AND OTHER PEDESTRIAN PATHS, PEDESTRIAN STREET CROSSINGS, CURB RAMPS, AND BLENDED TRANSITIONS.
- G36. WHERE PEDESTRIAN ACCESS ROUTES ARE CONTAINED WITHIN PEDESTRIAN STREET CROSSINGS WITHOUT YIELD OR STOP CONTROL, THE CROSS SLOPE OF THE PEDESTRIAN ACCESS ROUTE SHALL NOT EXCEED 4.5% DESIGN/5% CONSTRUCTED. CROSSWALKS ARE A TYPE OF PEDESTRIAN STREET CROSSING.
- G37. WHERE DRAINAGE IS HANDLED BY A CURB INLET IN LIEU OF A CATCH BASIN, PUBLIC WORKS HYDRAULICS ENGINEERING SHALL BE CONSULTED IF EXISTING CURB INLETS WILL BE AFFECTED BY CURB RAMP DESIGN.
- G38. CONCRETE GUTTER SHALL EXTEND 3' MINIMUM LENGTH BEYOND OUTSIDE OF CROSSWALK.
- G39. IN ACCORDANCE WITH ADA TITLE II REQUIREMENTS FOR NEW CONSTRUCTION AND ALTERATIONS (28 CFR PART 35, SECTION 35.151), PROWAG R202.3 STATES FOR ALTERATIONS "WHERE EXISTING ELEMENTS, SPACES, OR FACILITIES ARE ALTERED, EACH ALTERED ELEMENT, SPACE, OR FACILITY WITHIN THE SCOPE OF THE PROJECT SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS FOR NEW CONSTRUCTION." NOTE THAT FOR ALTERATIONS, IT MAY NOT BE POSSIBLE TO MEET ALL OF THE ACCESSIBILITY REQUIREMENTS. FOLLOW NEW CONSTRUCTION PROVISIONS TO THE EXTENT FEASIBLE. PROJECTS INVOLVING EXISTING STREETS AND SIDEWALKS MAY BE FURTHER CONSTRAINED BY LIMITED SPACE AND RIGHT-OF-WAY AVAILABILITY, UNDERLYING TERRAIN, UNDERGROUND STRUCTURES, DRAINAGE, AND OTHER FACTORS. THE GUIDELINES ALLOW DEPARTURES FROM NEW CONSTRUCTION CRITERIA IN ALTERATIONS WHERE EXISTING PHYSICAL CONSTRAINTS MAKE COMPLIANCE IMPRACTICABLE. THESE AND OTHER ALLOWANCES WILL GREATLY MITIGATE THE IMPACTS OF THE GUIDELINES.



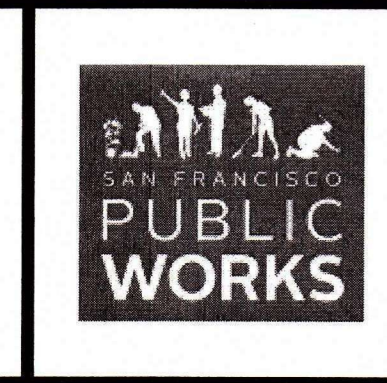
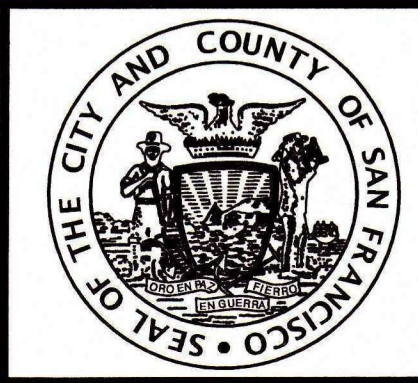
**CURB RAMP DEFINITION**



APPROVED: *Kevin M. Jara* 1/27/17  
 PUBLIC WORKS DISABILITY ACCESS COORDINATOR DATE:  
 EFFECTIVE DATE: 4/13/2017

NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				

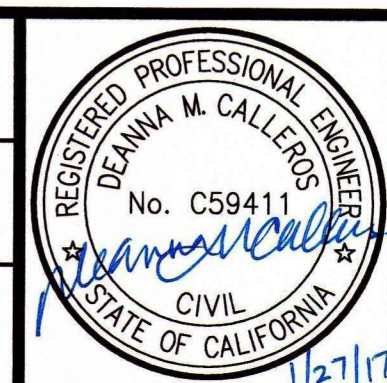
REFERENCE INFORMATION & FILE NO. OF SURVEYS
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DESIGN & ENGINEERING  
 PUBLIC WORKS  
 CITY & COUNTY OF SAN FRANCISCO  
 30 VAN NESS AVENUE, 5TH FLOOR  
 SAN FRANCISCO, CA 94102 - 8028

Acting Section Mgr: DEANNA CALLEROS  
*Deanna Calleros* 1/27/17  
 Division Mgr: PATRICK RIVERA  
*Patrick Rivera* 1/27/17  
 Acting City Engineer: JOHN THOMAS  
*John Thomas* 1/27/17

DESIGNED: DATE: PUBLIC WORKS 01/17
DRAWN: DATE: PUBLIC WORKS 01/17
CHECKED: DATE: PUBLIC WORKS 01/17



SCALE: NOT TO SCALE
SHEET OF SHEETS: 2 OF 11

**ACCESSIBLE STREET CROSSING STANDARD**

**STANDARD CURB RAMPS**  
**LEGEND, ABBREVIATIONS, AND GENERAL NOTES**

CONTRACT NO. NONE
DRAWING NO. RX-2
FILE NO. 102,855
REV. NO.