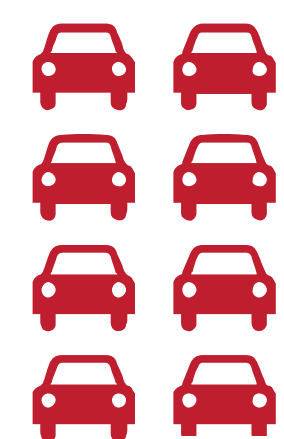


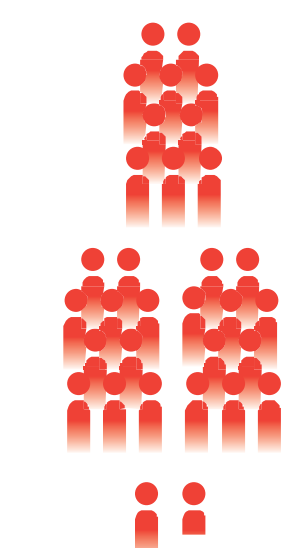
WHAT DO WE WANT ON OUR STREET?

The project could remove up to



79

parking spaces



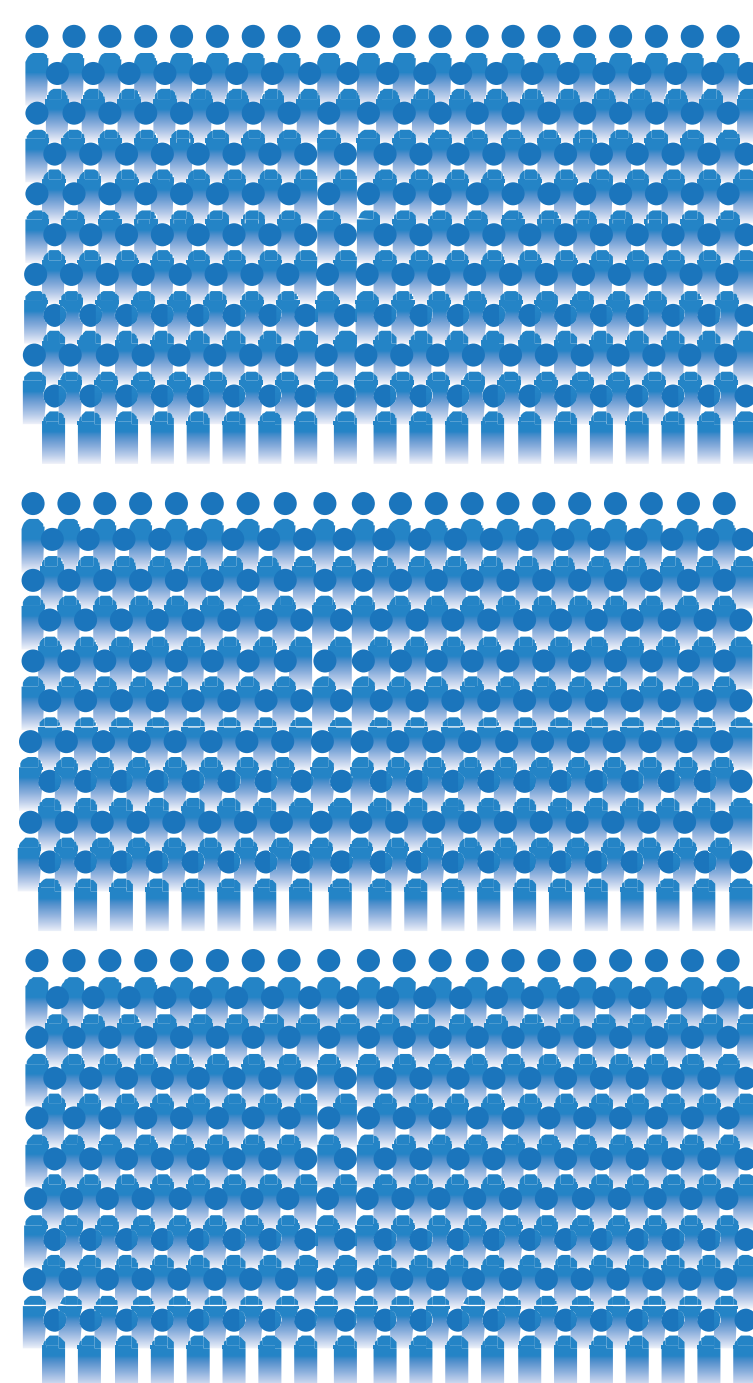
impacting as many as

316*

people each day

* assuming parking spaces turn over at a rate of four per day

= 10



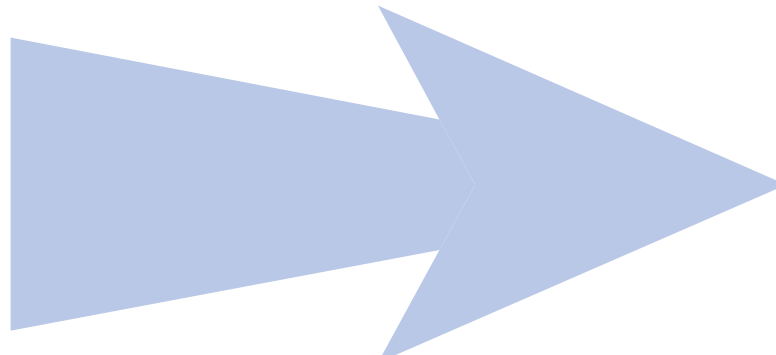
For comparison, about

3,000

people per day start or end their ride on Muni at a stop on Potrero Avenue



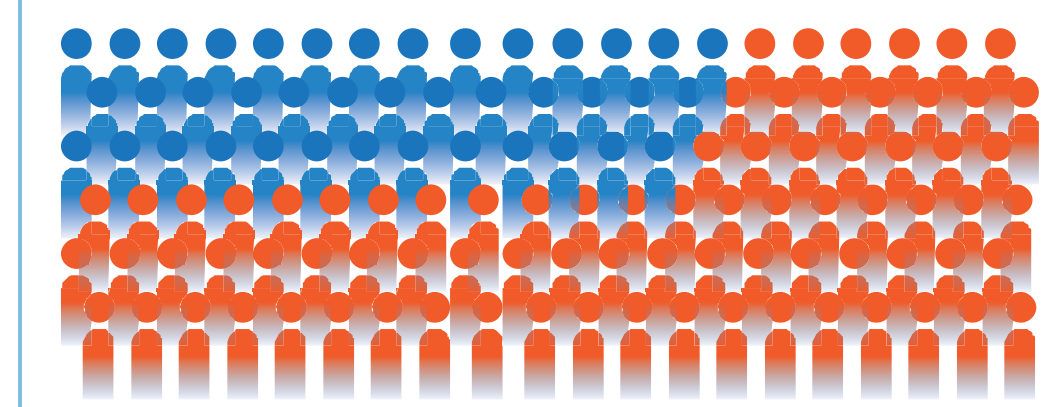
These people live or work in the area, are visiting the hospital or patronizing local businesses



SAVING MUNI CUSTOMERS' TIME

12,000

passengers ride Muni on Potrero Avenue each day



of which,

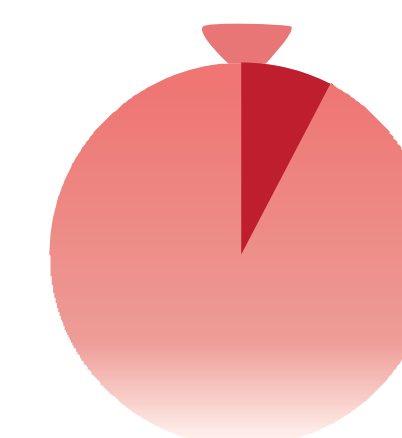
4,000

passengers ride the entire length of Potrero Avenue during the peak period

= 100

When completed, on average the project would save each person

3 minutes per round-trip



This adds up to over

13

hours saved per year for each commute



Each commuter would save almost a full day of travel time, which would add over

\$440,000

to the local economy every year in recovered time*

*using \$16.03/hr as the value of time
Source: 2013 MTC data

IMPROVING THE QUALITY OF MUNI SERVICE

On average, the 9 and 9L have

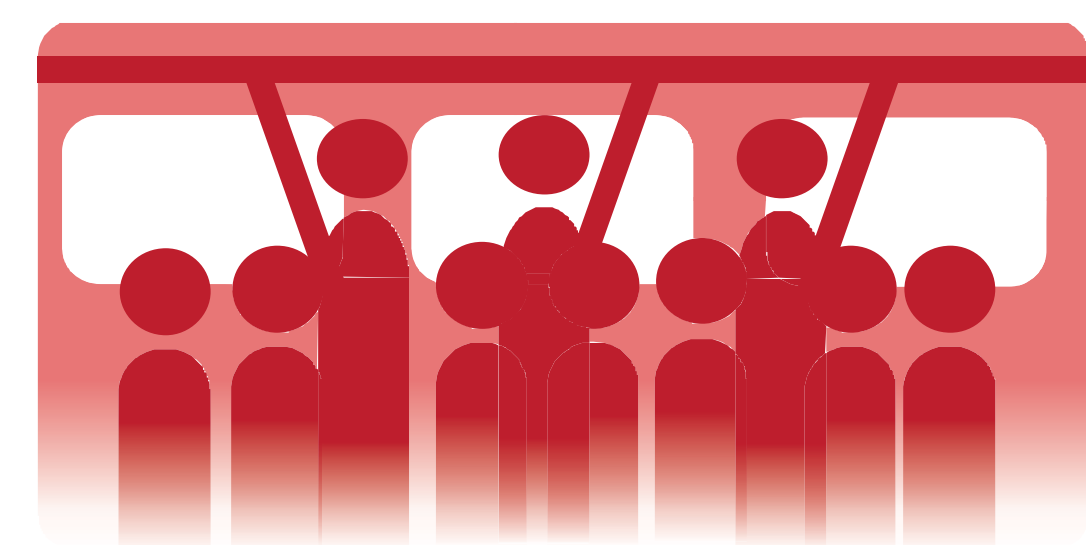
21

buses during the morning commute

and **20**

buses during the evening commute

which are **standing room only.**



After the project is completed,

buses will experience **fewer delays**

and will run **more frequently**

resulting in **more reliable service**

shorter wait times

and **less crowding**

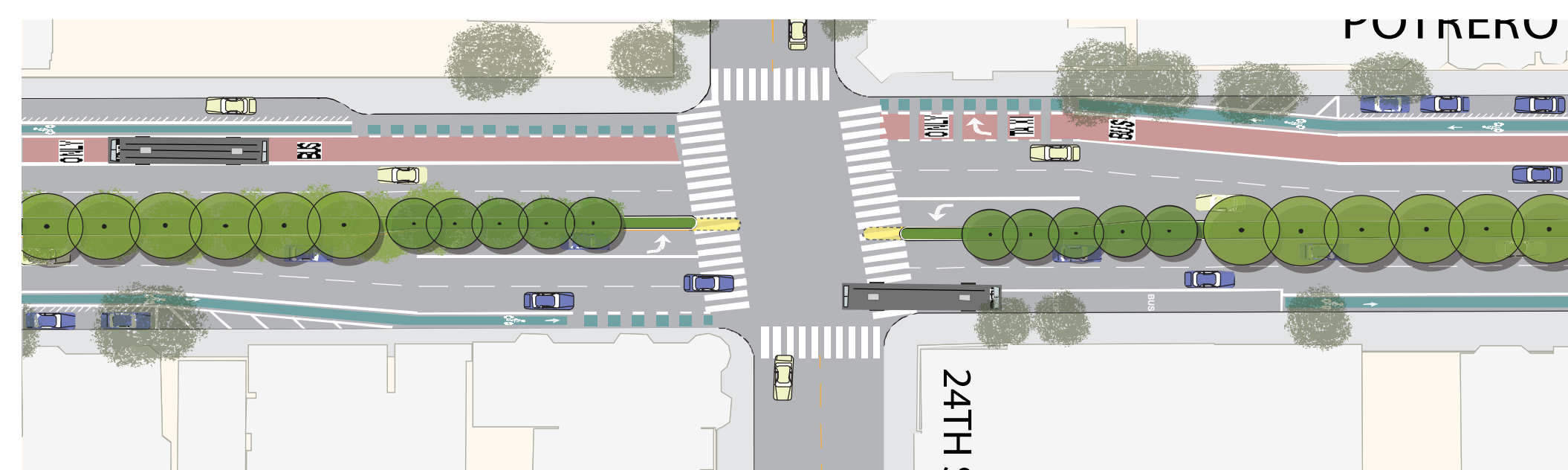


WALKING POTRERO, CROSSING MANY CULTURES



The project focuses on widening sidewalks in front of General Hospital to 14 feet, making Potrero a more walkable street

Artwork and designs representative of the community could be integrated into the sidewalks and crosswalks



The sidewalk widening will continue south on Potrero to 24th Street, linking the hospital with transit and with the vibrant 24th Street commercial district

POTRERO STREETScape IMPROVEMENTS

POTENTIAL BENEFITS



SEP 2013

ELEMENTS OF DESIGN

avored by July meeting attendees and survey respondents

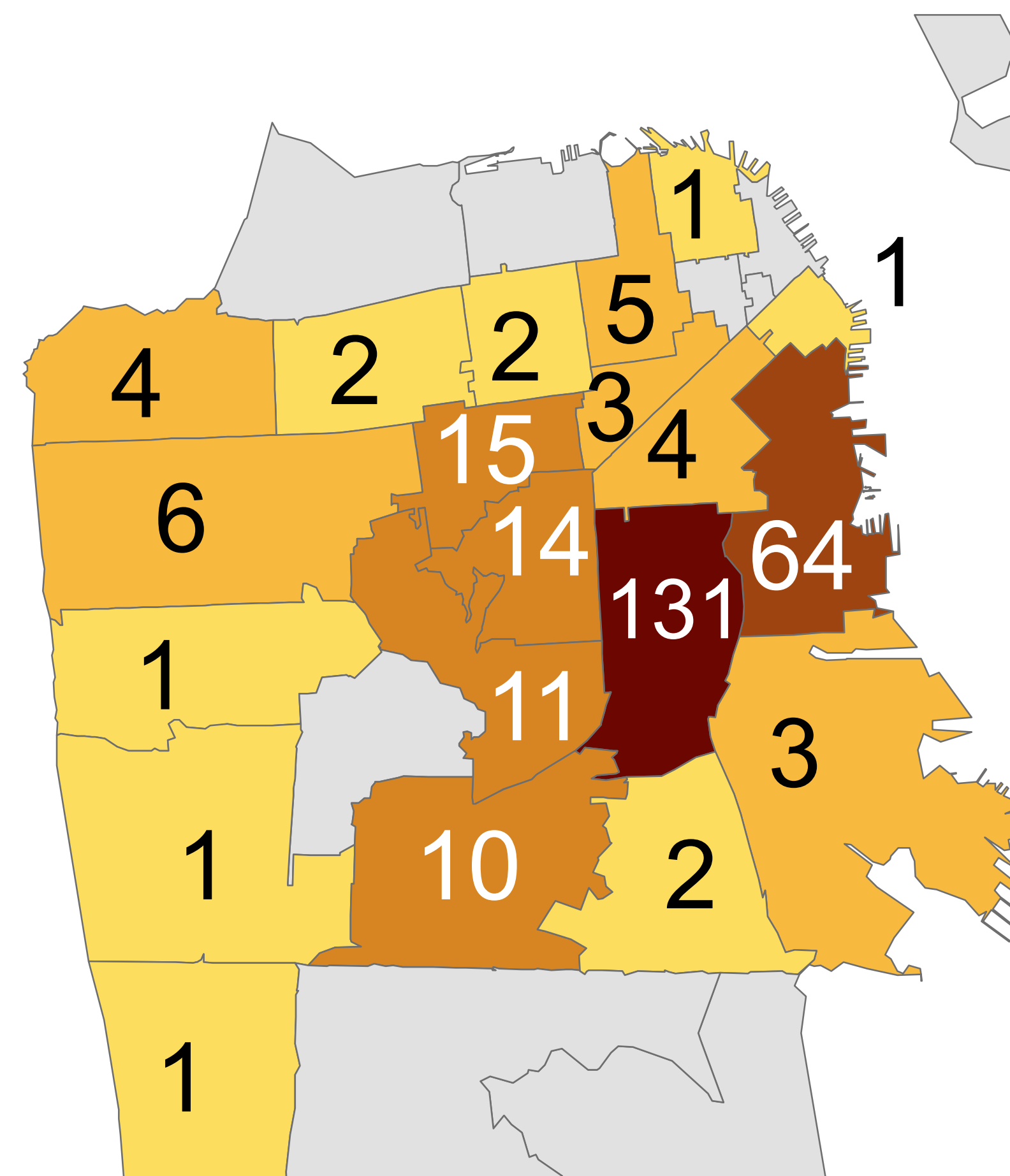
Desired Streetscape Design Elements

ordered from most favored to least



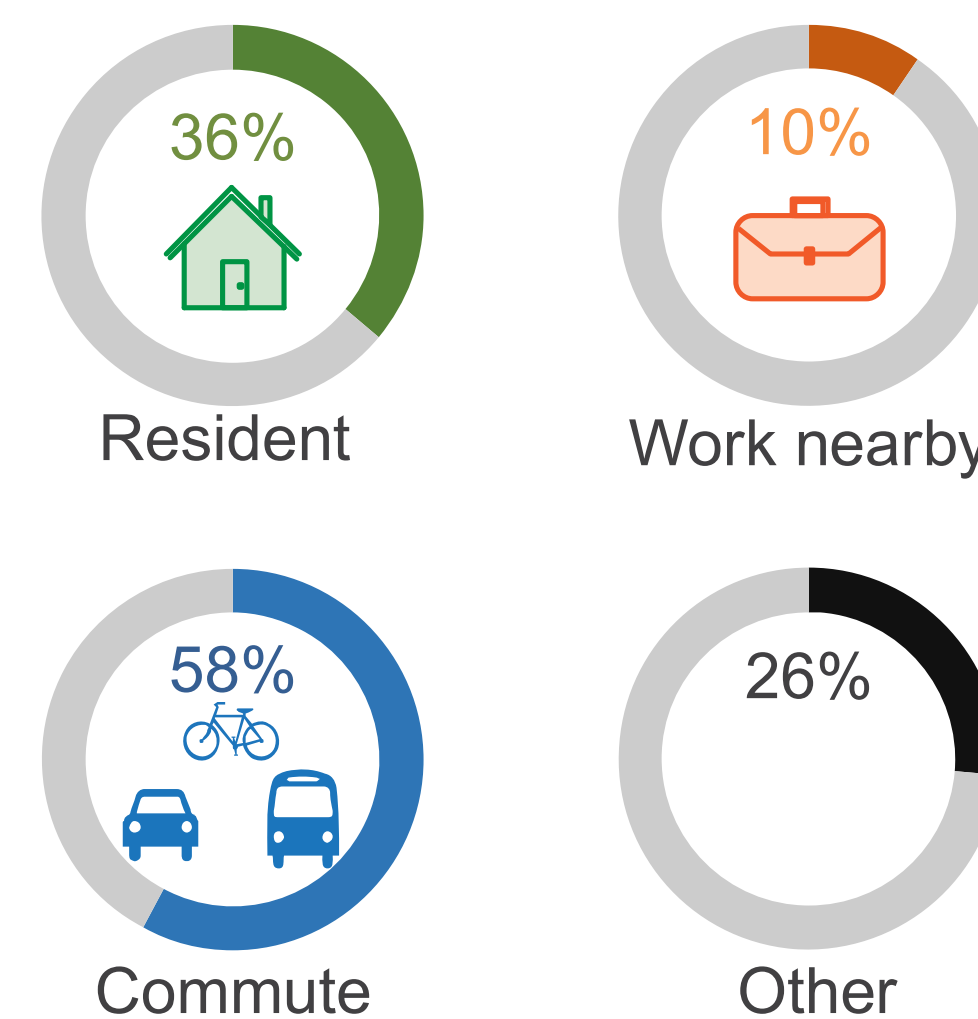
CHARACTERISTICS

of the community and stakeholders, based on survey



Number of responses per ZIP code
Total Responses: 289

What is your interest in Potrero?



CHANGES WE'VE MADE

in response to suggestions from the public



Sidewalk designs could be incorporated into the project to showcase the history of the community and its residents as well as provide a better walking experience along Potrero Avenue.



Sidewalk widening has been focused to the front of General Hospital and the Potrero Avenue / 24th Street transit hub. These improvements would provide a connection between the hospital and 24th Street.



The proposed streetscape improvements will now remove at most 79 spaces, compared to 105 spaces in the July proposal.



We are exploring opportunities to convert parallel parking into 90 degree parking, which could increase the number of parking spaces in the area.

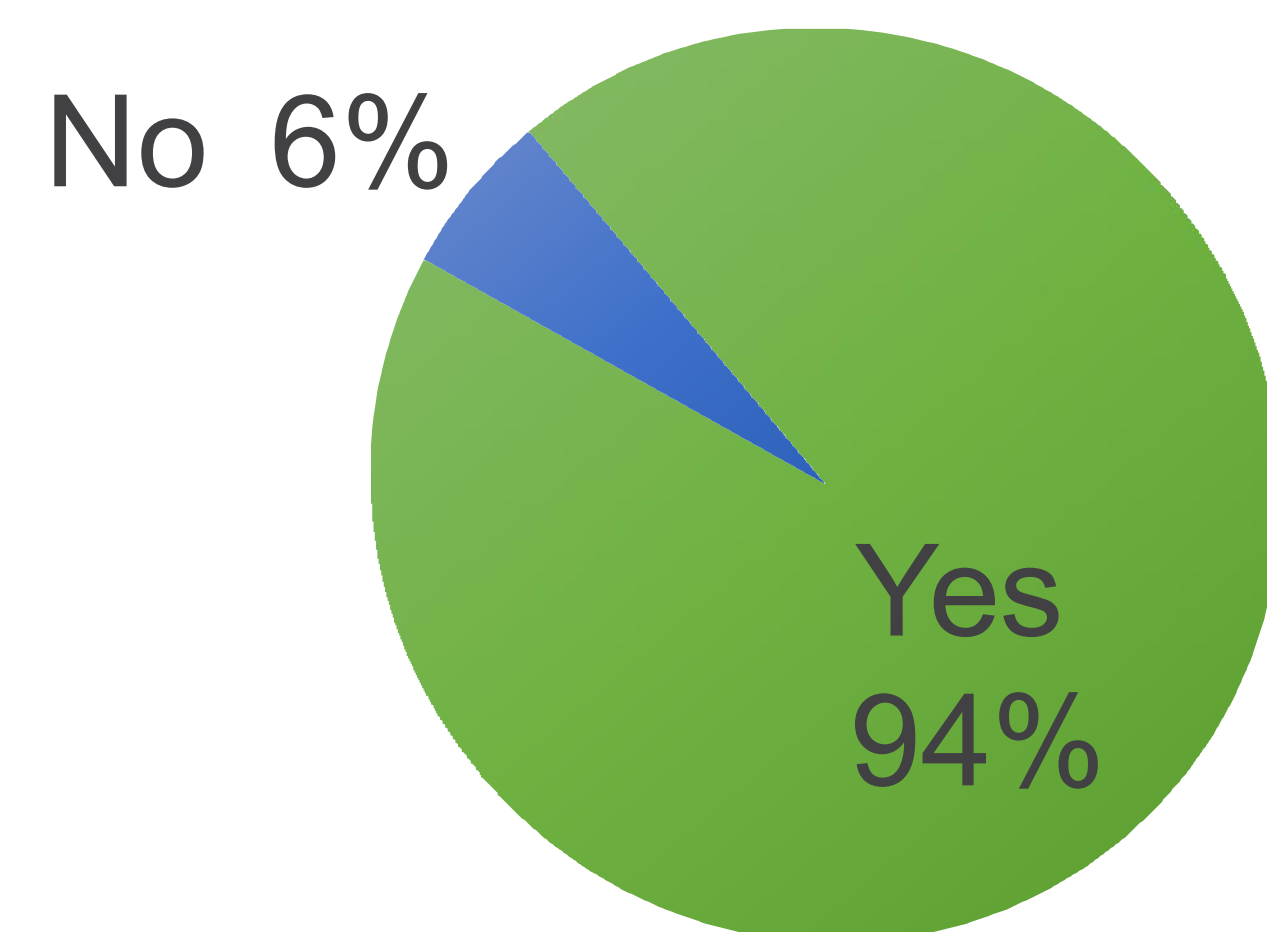


New concepts to improve bicycle safety in the corridor have been developed, including a northbound buffered bike lane option and green color treatments.

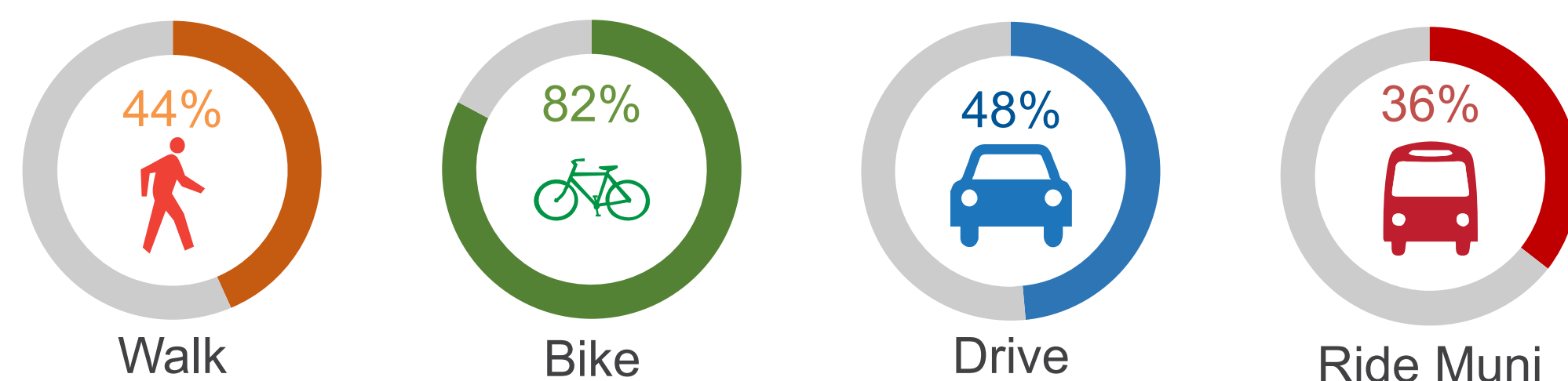


Bus stops have been reevaluated with accessibility in mind. Stops should be easier to reach compared to the July proposal.

In general, would you walk an extra block to a bus stop for a quicker, more reliable bus?



How do you travel along Potrero Avenue?



POTRERO STREETSCAPE IMPROVEMENTS

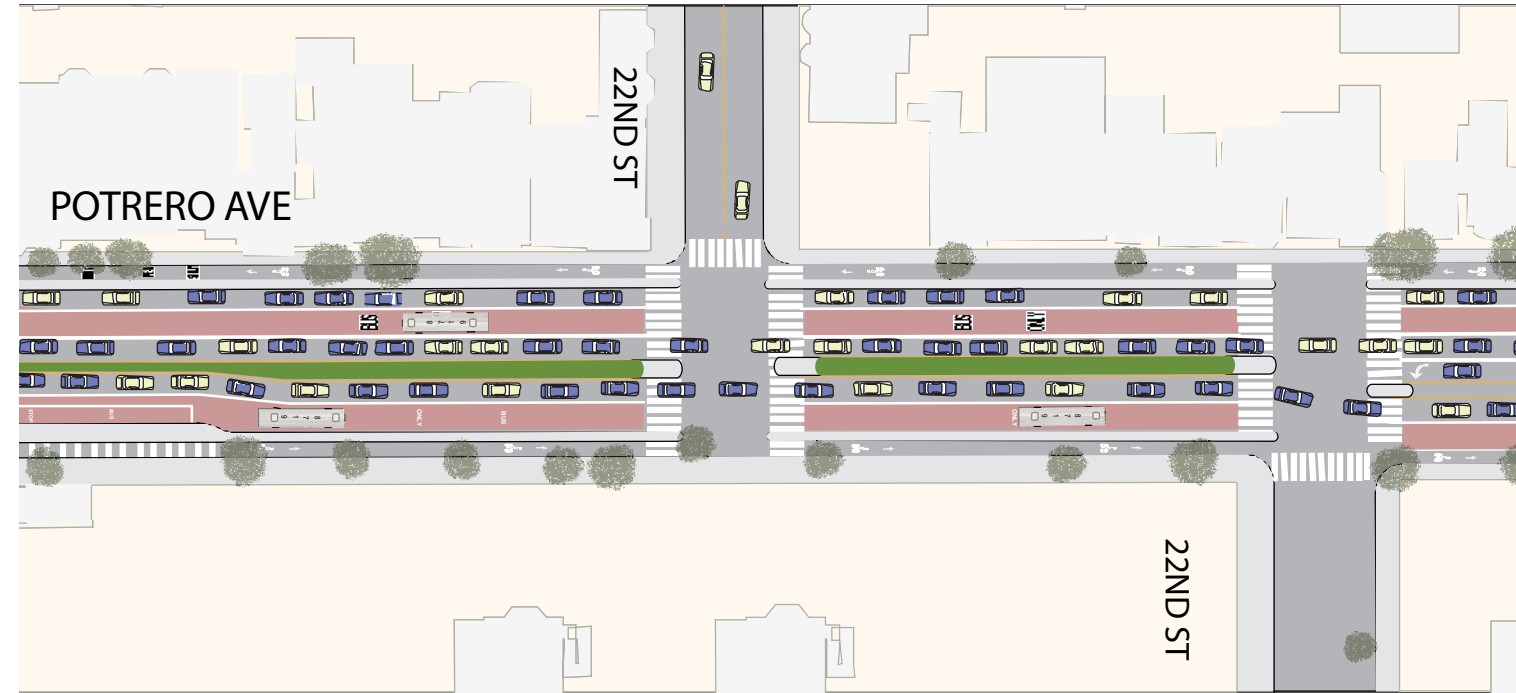
COMMUNITY SUGGESTIONS

ELIMINATED CONCEPTS

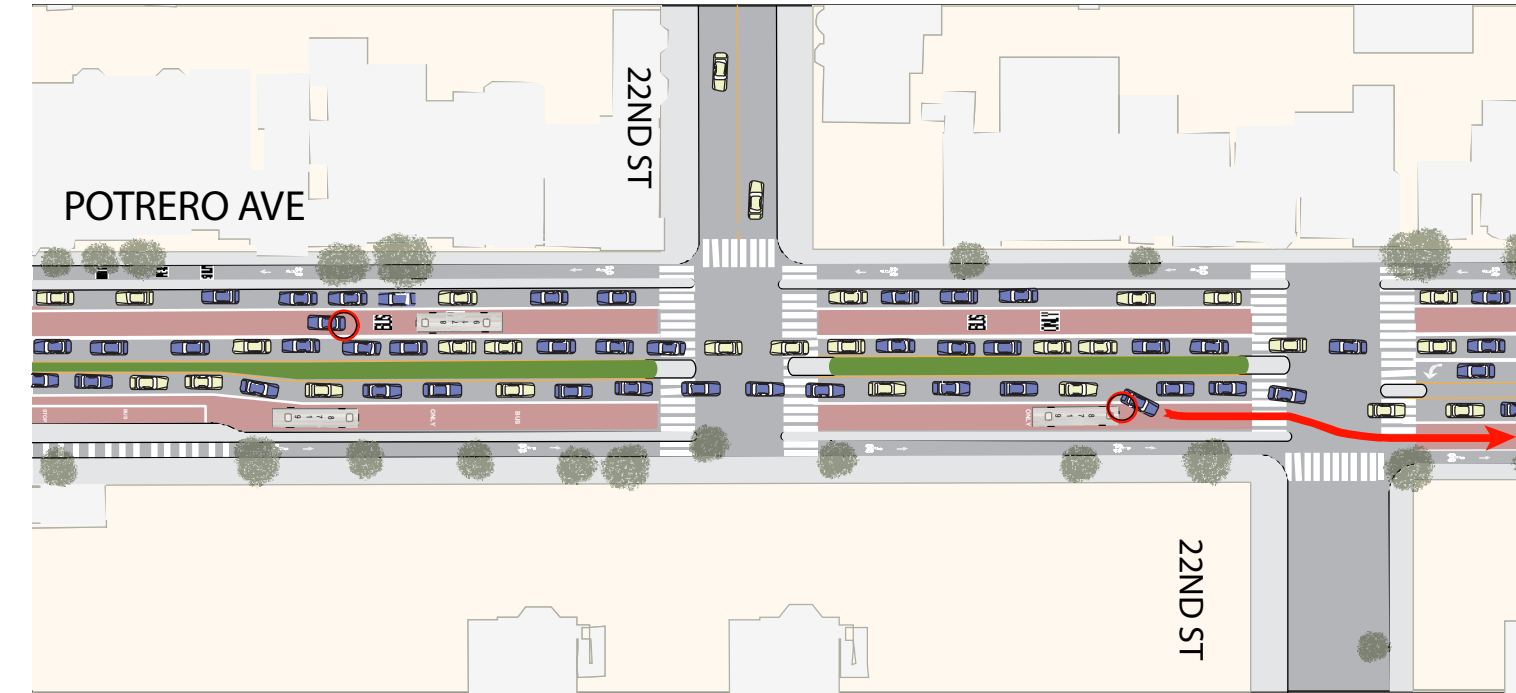
These ideas were evaluated and eliminated because they did not meet the project goals or had unacceptable impacts

REMOVING TRAFFIC LANES

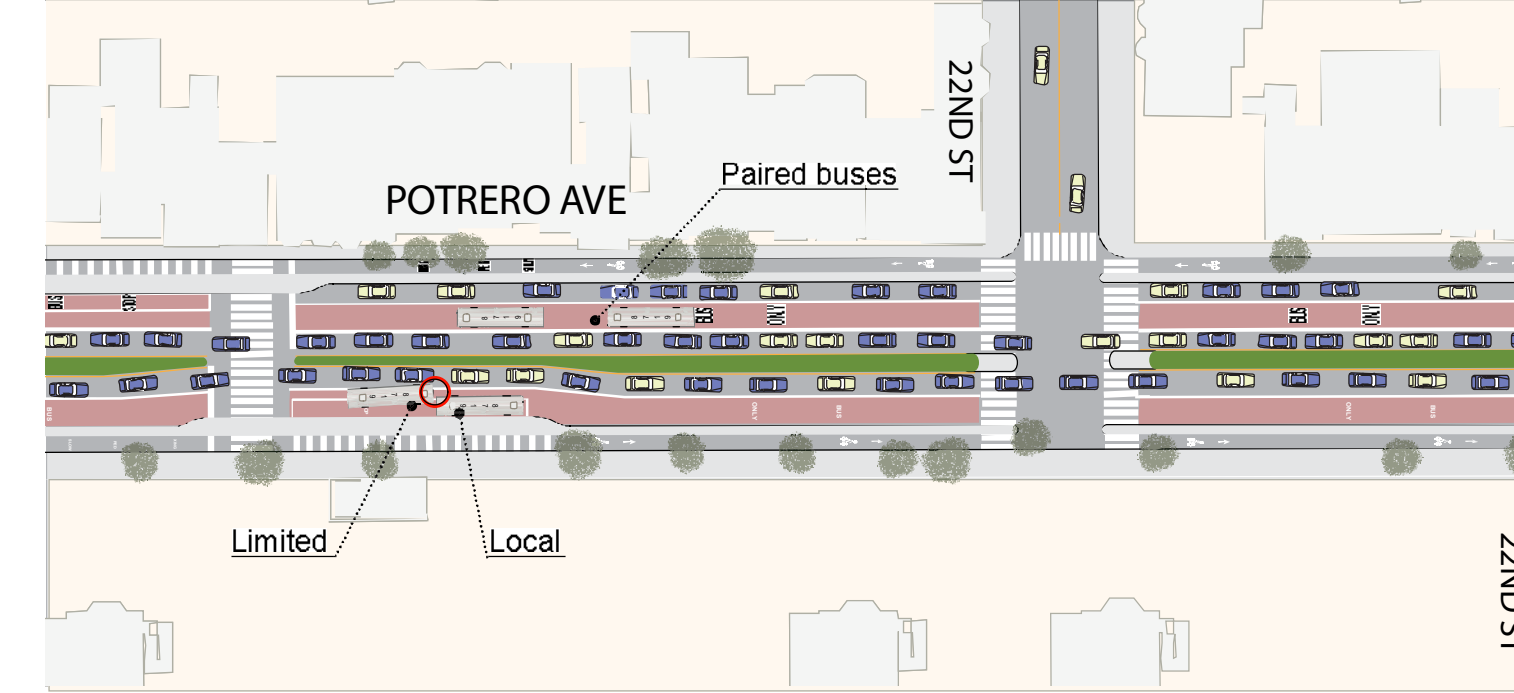
With one traffic lane in each direction, traffic would back up more often, and the traffic jams that already exist on Potrero would become more severe.



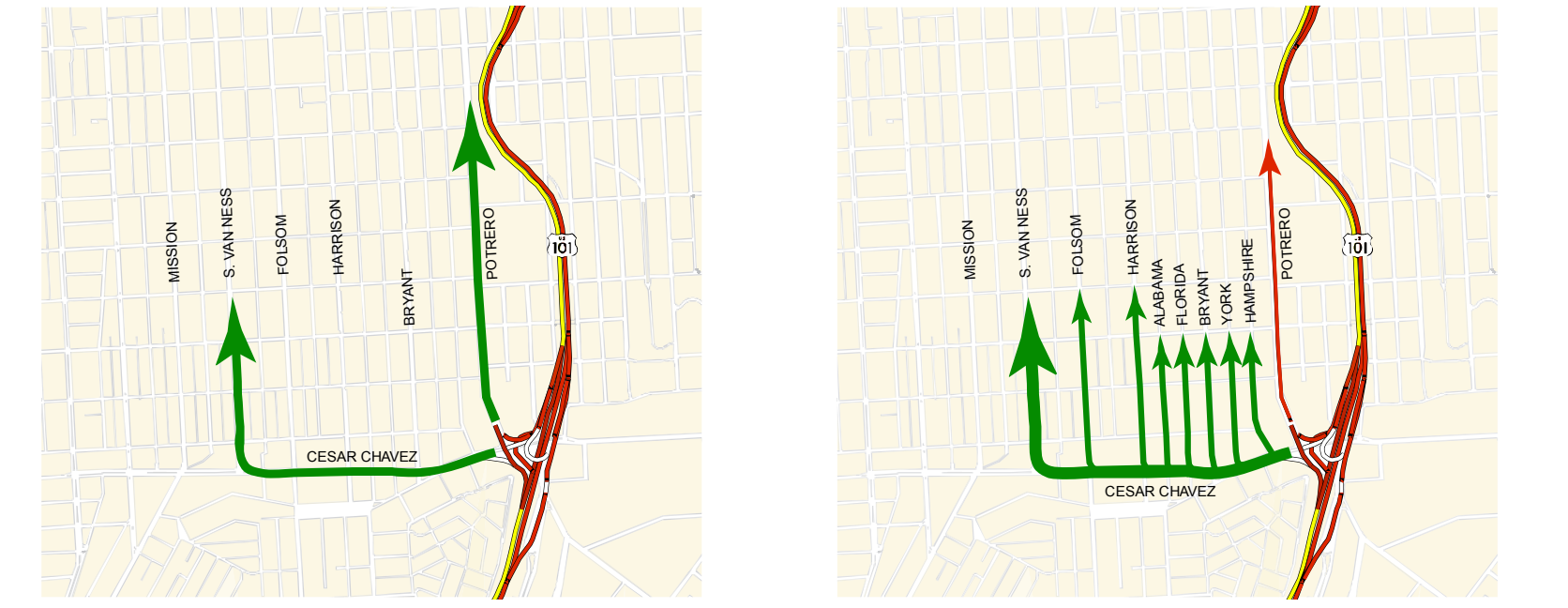
As traffic backs up on Potrero, people would be more likely to violate the transit only lanes and slow down Muni, like they do on Mission St downtown.



In this situation, the 9 Limited bus would not have room to use a traffic lane to pass local buses, making the limited service no faster than the local.



As the drive on Potrero becomes more difficult, people would seek alternate routes through the Mission if 101 is congested. These routes would likely use local neighborhood streets, some of which have already had traffic calming measures put into place at the request of residents.



REMOVING MEDIANS

Medians are needed to provide space in the roadway for pedestrian refuges and left turn pockets. Both options being considered would provide a wider, concrete median at all intersections between 21st and 25th Streets.



Photo: Google Maps | 2013

Potrero is a wide street, and those with lower mobility may not be able to cross the entire intersection at once. These people need a refuge in the center of the street that the median provides.

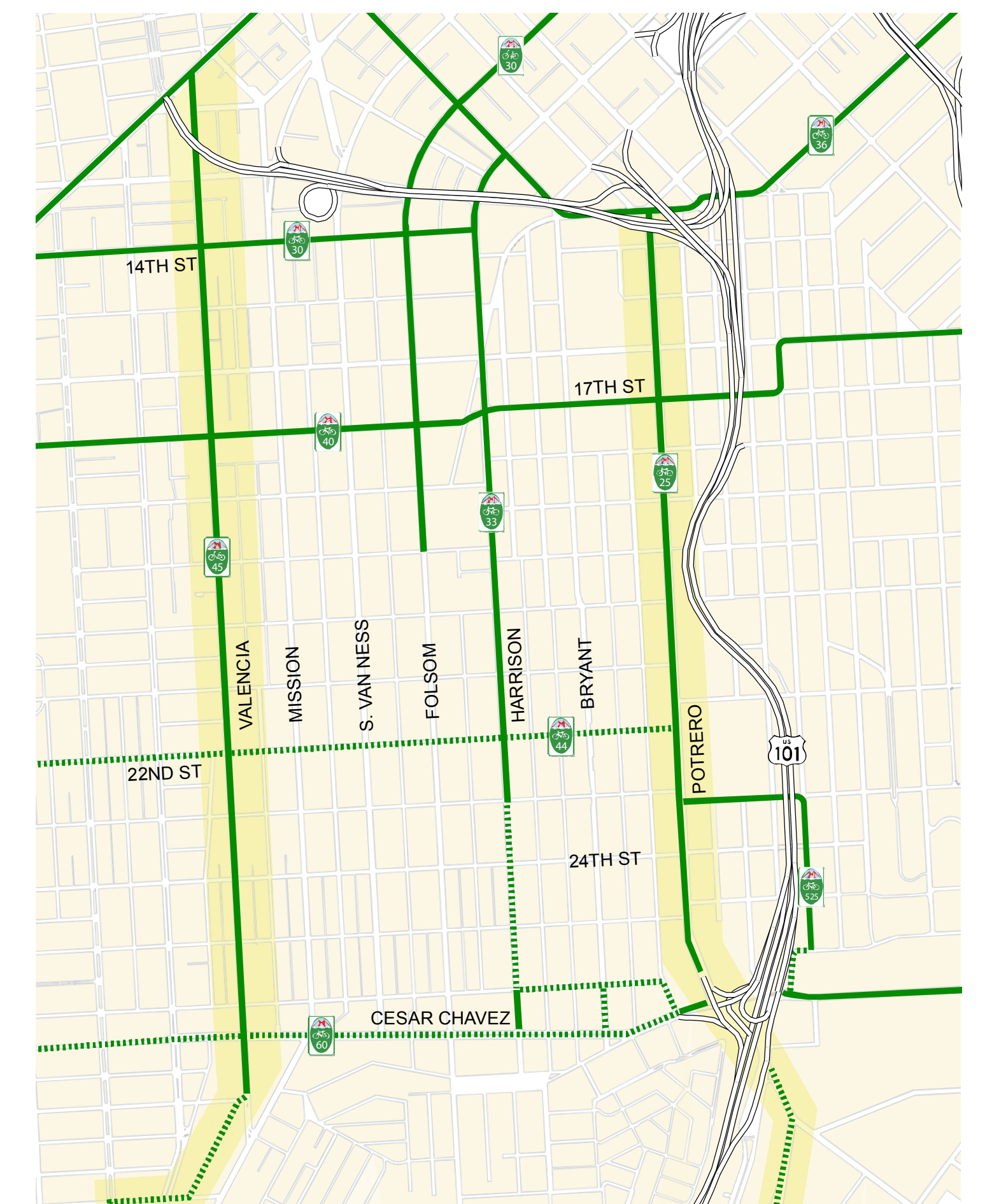
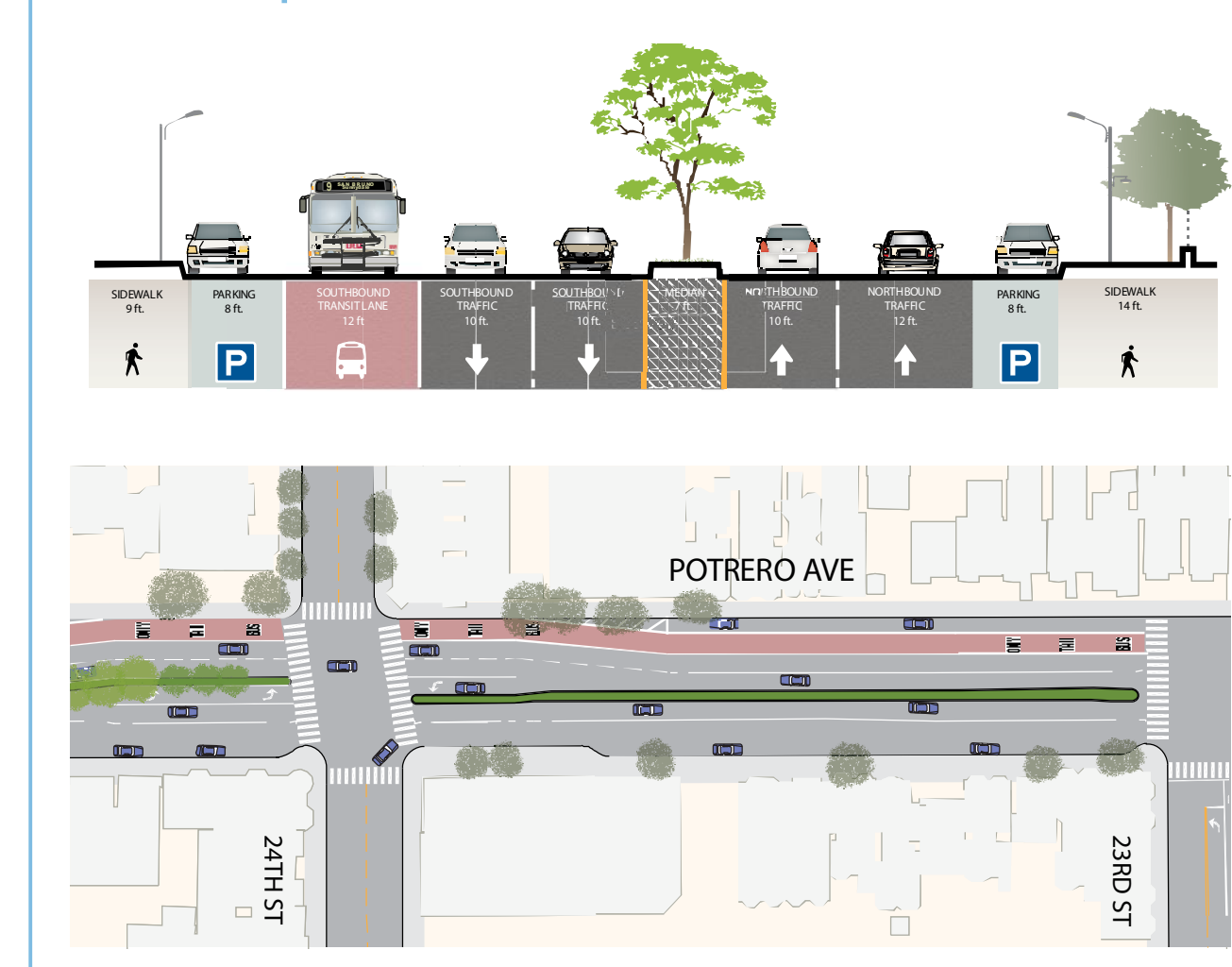
Medians also provide an opportunity to landscape the street with trees and other greenery to improve the overall aesthetic environment of Potrero.

REMOVING BIKE LANES

The idea of removing bike lanes was eliminated because Potrero is the only street besides Valencia that provides a direct connection between the Mission and the southern neighborhoods.

As shown on the right, Potrero and Valencia are the only through north-south bike routes between Twin Peaks and Highway 101. Bike routes on Potrero Avenue are needed to make cycling more safe, comfortable, and convenient.

Concept art of Potrero without bike lanes



POTRERO STREETScape IMPROVEMENTS

OTHER OPTIONS CONSIDERED



SEP 2013